



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

Jay Morgan
2025.12.02 18:19:05 -05'00'

PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED
AND SEALED. THE SIGNATURE MUST BE VERIFIED ON THE
ELECTRONIC DOCUMENTS.

TENNESSEE DEPARTMENT OF TRANSPORTATION
7345 REGION LANE
KNOXVILLE, TN 37914
JAMES MORGAN, P.E. NO. 111658

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING
SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN2
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS	1A
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B, 2B1
SPECIAL NOTES	2D
DETAILS SHEET	2G
RAMP DETAILS	5, 5B, 5D, 5H

YEAR	PROJECT NO.	SHEET NO.
2025	NH-I-275-3(137)	ROADWAY-SIGN2

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**SIGNATURE
SHEET**

TYPE	YEAR	PROJECT NO.	sheet no.
RESURF.	2025	NH-I-275-3(137)	1A

REV. 12-02-25: REVISED INDEX.

ROADWAY INDEX

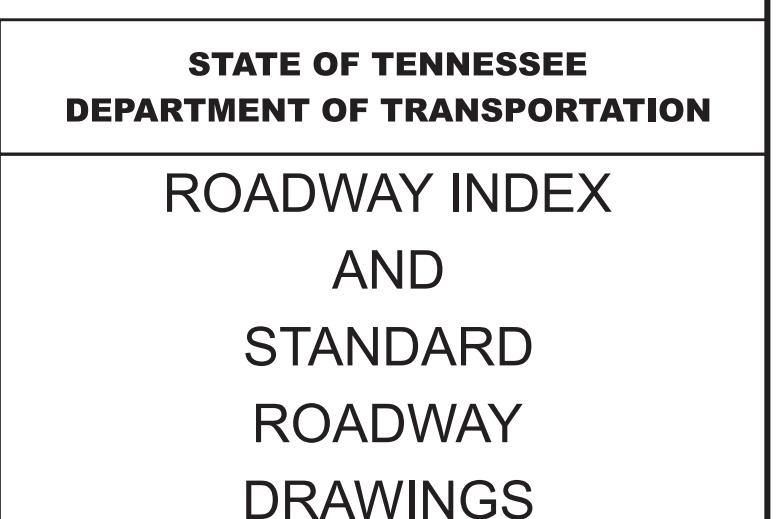
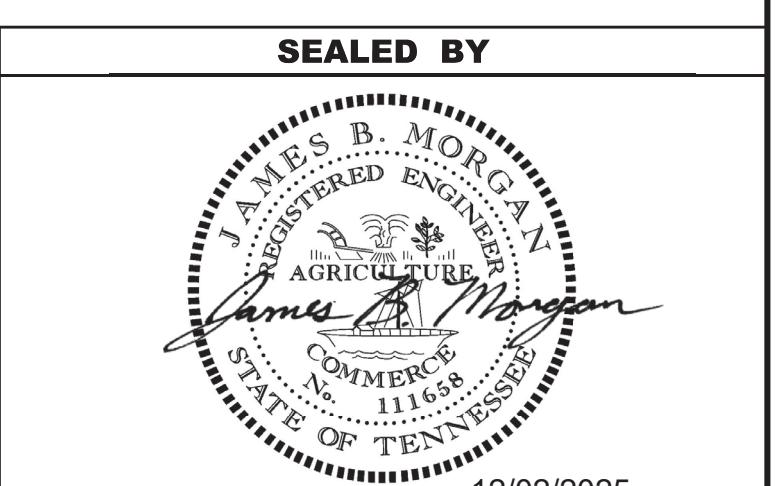
STANDARD ROADWAY DRAWINGS

SHEET NAME	SHEET NO.	DWG.	REV.	DESCRIPTION
SIGNATURE SHEETS	ROADWAY-SIGN1,2			STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS
TITLE SHEET	1	RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS	1A	RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
STANDARD TRAFFIC DESIGN DRAWINGS	1A1	RD-L-1	02-20-20	STANDARD LEGEND
ESTIMATED ROADWAY QUANTITIES	2	RD-L-1A		STANDARD LEGEND
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B, 2B1			
GENERAL NOTES	2C			
SPECIAL NOTES	2D, 2D1	RD11-SE-1		TRANSITION AND CROSS SLOPE DETAILS
ENVIRONMENTAL NOTES	2E	RD11-SE-3		SUPERELEVATION TRANSITION DETAILS FOR DIVIDED ROADWAYS
EROSION PREVENTION AND SEDIMENT CONTROL NOTES	2E1	RD11-SE-3A		SUPERELEVATION TRANSITION SECTIONS FOR DIVIDED ROADWAYS
TABULATED QUANTITIES	2F	RD11-LR-2		MINIMUM RUNOFF LENGTHS (LR) FOR RURAL HIGHWAYS
DETAIL SHEETS	2G			
UTILITY NOTES AND UTILITY OWNERS	3			
RAMP DETAILS	5-5K	RP-J-25	05-01-20	CONCRETE PAVEMENT JOINT REPAIR
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL	T1			

NOTE: THE ALPHABETICAL LETTERS "I", "O" & "Q" ARE NOT USED IN NUMBERING OF SHEETS.

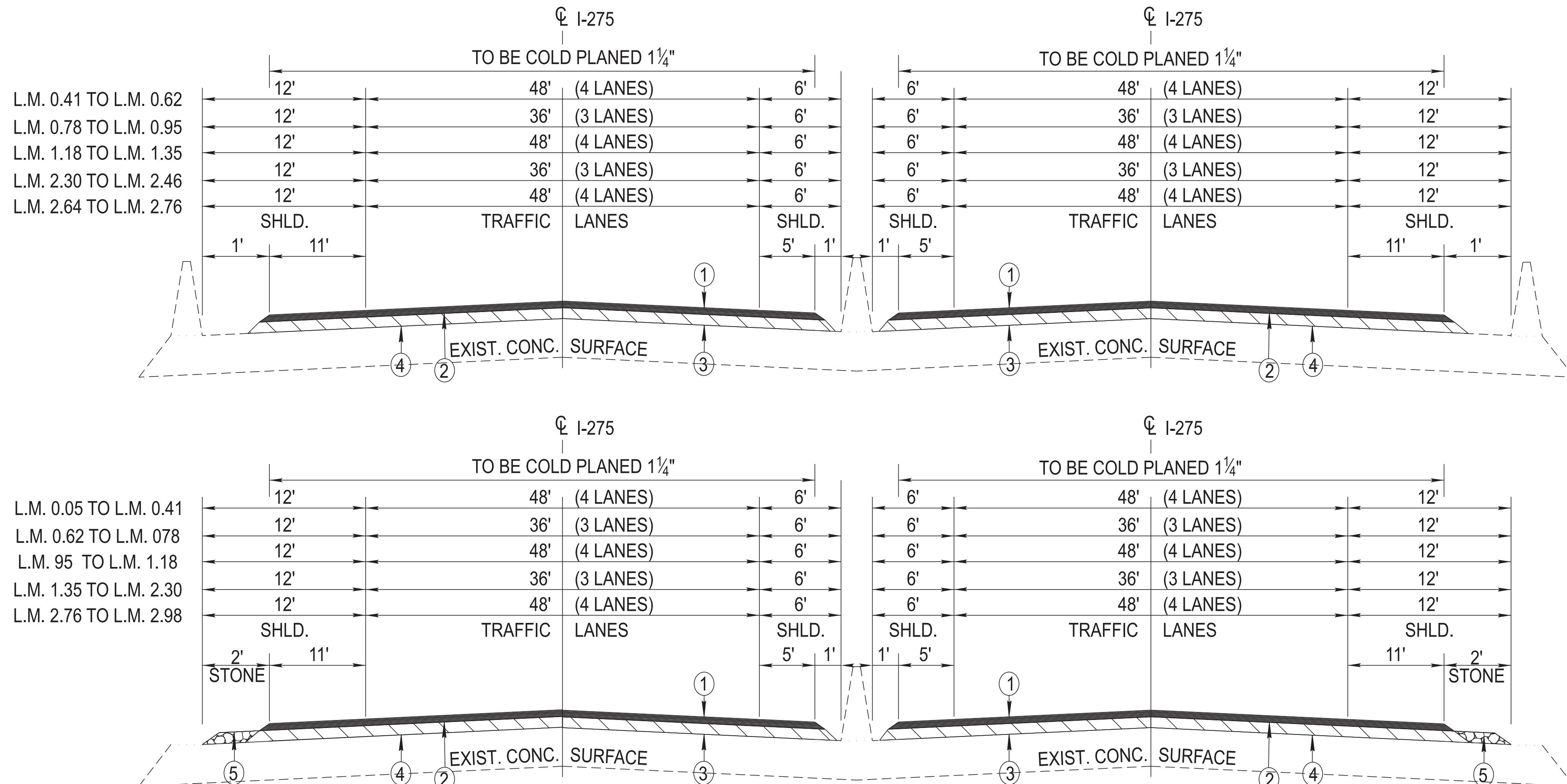
NO PROJECT COMMITMENT SHEET INCLUDED IN THIS SET OF PLANS

NO UTILITY SHEETS ARE INCLUDED IN THIS PLAN SET



ESTIMATED ROADWAY QUANTITIES				FOOTNOTES		TYPE			YEAR		PROJECT NO.		SHEET NO.															
ITEM NO.	DESCRIPTION	UNIT	QUANTITY	FOOTNOTES																								
				471275-F8-005																								
(10)	201-01 CLEARING AND GRUBBING	LS	1	(1)	TO BE USED ON THE SHOULDER OF INTERSECTION RAMPS.																							
	208-01.05 BROOMING & DEGRASSING SHOULDERS	L.M.	13	(2)	120 TONS TO BE USED FOR PLANT STARTUP AND HEATING UP EQUIPMENT AT BEGINNING OF EACH SHIFT, 15 TONS WASTE MATERIAL PER DAY.																							
(1)	303-01 MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	1059	(3)	SEE SHEET 2F FOR SIGN TABULATED QUANTITIES. THE CONSTRUCTION SIGNING IS TO BE A MINIMUM. OTHER SIGNS MAY BE REQUIRED AS DIRECTED BY THE TDOT MANAGER.																							
	307-03.12 ASPHALT CONCRETE MIX (PG76-22) GRADING CM	TON	24291	(4)	ITEM TO BE USED ONLY WHEN A REDUCED SPEED LIMIT IS ESTABLISHED WITHIN THE PROJECT CONSTRUCTION WORK ZONE LIMITS. ITEM INCLUDES SIGN FACE, SUPPORTS AND TWO (2) TYPE "B" FLASHERS PER STANDARD SPECIFICATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TURNING ON THE TYPE "B" FLASHERS WHEN WORKERS ARE IN THE CONSTRUCTION WORK ZONE AND TURNING THEM OFF WHEN WORKERS ARE NO LONGER IN THE CONSTRUCTION WORK ZONE.																							
(8)	403-02.01 TRACKLESS TACK COAT	TON	183	(5)	ANY DAMAGE THAT OCCURS DURING REMOVAL SHALL BE REPAIRED AT THE CONTRACTORS EXPENSE AND TO THE SATISFACTION OF THE TDOT MANAGER.																							
(9)	403-02.02 HOT APPLIED TACK COAT	TON	163	(6)	THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMANCE PLASTIC FOR THERMO PLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME PRICE AS BID FOR THERMOPLASTIC.																							
	411-03.10 ACS MIX(PG76-22) GRADING D	TON	2816	(7)	TO BE USED FOR TEMPORARY PAVEMENT MARKING ON INTERMEDIATE LAYERS.																							
(2)	411-03.23 ACS MIX (PG76-22) OGFC	TON	9792	(8)	USE AN APPROVED TRACKLESS TACK FROM QPL 40-F APPLIED WITH A DISTRIBUTOR AT A MINIMUM RATE OF 0.20 GAL/SY (APPROXIMATE RESIDUAL RATE 0.10 GAL/SY). PAVING OF THE OGFC SHALL NOT BEGIN UNTIL THE DEPARTMENT IS SATISFIED THE APPLICATION RATE IS ACHIEVED AND THE EMULSION HAS FULLY BROKEN. MULTIPLE PASSES MAY BE REQUIRED. OR, EMULSION TYPE CQS-1HP MAY BE APPLIED WITH A SPRAY PAVER AT AN APPLICATION RATE BETWEEN 0.18 TO 0.23 GAL/SY. THE SPRAY PAVER SHALL BE A SINGLE PIECE OF EQUIPMENT THAT APPLIES THE TACK COAT AND SPREADS THE BITUMINOUS PAVEMENT. AT A MINIMUM THE SPRAY PAVER SHALL MEET THE PAVER REQUIREMENTS OF 407.06 AND THE DISTRIBUTOR REQUIREMENTS IN 402.03.																							
	411-12.01 SCORING SHOULDERS (CONTINUOUS) (16IN WIDTH)	L.M.	12	(9)	TO BE USED FOR TACK COAT UNDERNEATH CM. HOT APPLIED TRACKLESS TACK MAY BE EITHER HOT APPLIED ASPHALT BINDER OR AN APPROVED HOT APPLIED TRACKLESS TACK COAT FROM QPL 40, SECTION F. IF USING ASPHALT BINDER THE MINIMUM GRADE SHALL BE PG64-22 BUT A HIGHER GRADE MAY BE USED AT THE CONTRACTOR'S DISCRETION.																							
(16)	415-01.01 COLD PLANING BITUMINOUS PAVEMENT	TON	33723	(10)	SEE DETAIL #3 ON SHEET 2G.																							
	502-03.05 HOT APPLIED FIBER-POLYMER PATCHING MATERIAL	LB.	100,000	(11)	TO BE USED FOR THE EXIT RAMPS, REFER TO T-M-6.																							
	502-08.10 SAWING & RESEALING JOINTS (SILICONE SEALANT)	L.F.	127086	(12)	TO BE USED FOR THE ENTRANCE RAMPS, REFER TO T-M-6.																							
	712-01 TRAFFIC CONTROL	LS	1	(13)	CONTRACTOR SHALL USE THE EXTRUDED OR RIBBON METHOD FOR APPLICATION.																							
	712-04.01 FLEXIBLE DRUMS (CHANNELIZING)	EACH	366	(14)	THE CONTRACT REQUIRES A QUEUE PROTECTION TRUCK WHEN LANES ARE REDUCED WITHIN THE PROJECT LIMITS. TDOT WILL NOT MAKE PAYMENTS FOR MORE THAN 1 UNIT DAY PER SCHEDULED SHIFT REGARDLESS OF THE 12-HOUR LIMITATION DEFINED PER SPECIAL PROVISION 712 PTQ UNLESS MORE THAN 1 LOCATION IS REQUESTED AND APPROVED. SHOULD THE CONTRACTOR REQUIRE ADDITIONAL QUEUE PROTECTION BEYOND THE QUANTITIES ESTABLISHED IN THE CONTRACT PLANS, THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING QUEUE PROTECTION AS DEFINED IN SP 712PTQ AT NO ADDITIONAL COST.																							
	712-05.01 WARNING LIGHTS (TYPE A)	EACH	183	(15)	TO BE USED FOR AUXILIARY LANES AT CLOSELY SPACED RAMPS, REFER TO T-M-6.																							
(3)	712-06 SIGNS (CONSTRUCTION)	S.F.	2420	(16)	TO BE USED TO REPAIR EXISTING CONCRETE AFTER COLD PLANING.																							
(4)	712-06.16 SIGNS (CONSTRUCTION)(REDUCED SPEED WARNING)	EACH	4																									
	712-08.03 ARROW BOARD (TYPE C)	EACH	4																									
	712-08.08 SPEED FEEDBACK SIGN ASSEMBLY	EACH	2																									
	712-08.09 DIGITAL SPEED LIMIT SIGN ASSEMBLY	EACH	2																									
(14)	712-08.12 QUEUE PROTECTION TRUCK	DAY	50																									
	713-16.01 CHANGEABLE MESSAGE SIGN UNIT	EACH	6																									
	716-01.23 SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR)(2 COLOR)	EACH	1423																									
	716-01.30 REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	1423																									
	716-02.04 PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.	2000																									
	716-02.07 PLASTIC PAVEMENT MARKING (24" BARRIER LINE)	L.F.	3340																									
	716-04.07 PLASTIC PAVEMENT MARKING (EXIT ONLY ARROW)	EACH	15																									
	716-05.49 PAINTED PAVEMENT MARKINGS(8" LINE)	L.M.	26																									
	716-12.02 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	13																									
	716-12.05 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN DOTTED LINE)	L.F.	1458																									
	716-12.06 ENHANCED FLAT LINE THERMO (8IN LINE																											

REV. 12/02/25: UPDATED TYPICAL SECTIONS,
PAVEMENT TABULATION AND BRIDGE NOTES.



NOTES: THE EXISTING ASPHALT IS PAVED 1.25" ABOVE THE EXISTING CONCRETE.
THE MAINLINE FROM L.M. 0.00 TO L.M. 2.98 WILL BE PAVED WITH "D" MIX.

BRIDGE NOTES:
BRIDGE OVER BERNARD AVE (L.M. 0.39) IS TO REMAIN AS IS. REFER TO DETAIL 2 ON SHEET 2G FOR REFERENCE TO TRANSITION DOWN TO THE CONCRETE BRIDGE DECK.

UNDERPASS AT BAXTER AVE (L.M. 0.68) IS TO BE COLD PLANED $1\frac{1}{4}$ " AND PAVED WITH $1\frac{1}{4}$ " 411-03.23 ACS MIX (PG76-22) OGFC. SEE DETAIL 1 ON SHEET 2G.

BRIDGE OVER OLDHAM AVE (L.M. 1.02 LT.) IS TO BE COLD PLANED 1 $\frac{1}{4}$ " AND PAVED WITH 1 $\frac{1}{4}$ " 411-03 10 ACS MIX (PG76-22) GRADING "D" SEE DETAIL 2 ON SHEET 2G

BRIDGE OVER WOODLAND AVE (L.M. 1.02 RT.) IS TO BE COLD PLANED $1\frac{1}{4}$ " AND PAVED WITH $1\frac{1}{4}$ " 411-03.10 ACS MIX (PG76-22) GRADING "D". SEE DETAIL 2 ON SHEET 2G.

BRIDGE OVER TENNESSEE AVE (L.M. 1.67) IS TO BE COLD PLANED $1\frac{1}{4}$ " AND PAVED WITH $1\frac{1}{4}$ " 411-03.10 ACS MIX (PG76-22) GRADING "D". SEE DETAIL 2 ON SHEET 2G.

BRIDGE OVER SOUTHERN RR (L.M. 1.93) IS TO BE COLD PLANED $1\frac{1}{4}$ " AND PAVED WITH $1\frac{1}{4}$ " 411-03.10 ACS MIX (PG76-22) GRADING "D". SEE DETAIL 2 ON SHEET 2G.

BRIDGE OVER SECOND CREEK (L.M. 2.17) IS TO BE COLD PLANED $1\frac{1}{4}$ " AND PAVED WITH $1\frac{1}{2}$ " 307-03.12 ASPHALT CONCRETE MIX (PG76-22) (BPMB-HM) GRADING CM AND $1\frac{1}{4}$ " 411-03.23 ACS MIX (PG76-22) OGFC.

UNDERPASS AT HEISKELL AVE (L.M. 2.23) IS TO BE COLD PLANED $1\frac{1}{4}$ " AND PAVED WITH $1\frac{1}{4}$ " 411-03.23 ACS MIX (PG76-22) OGFC. SEE DETAIL 1 ON SHEET 2G.

BRIDGE OVER SECOND CREEK (L.M. 2.46) IS TO BE COLD PLANED $1\frac{1}{4}$ " AND PAVED WITH $1\frac{1}{2}$ " 307-03.12 ASPHALT CONCRETE MIX (PG76-22) (BPMB-HM) GRADING CM AND $1\frac{1}{4}$ " 411-03.23 ACS MIX (PG76-22) OGFC.

UNDERPASS AT RAMP TO SR-9 (L.M. 2.86) IS TO BE COLD PLANED $1\frac{1}{4}$ " AND PAVED WITH $1\frac{1}{4}$ " 411-03.23 ACS MIX (PG76-22) OGFC. SEE DETAIL 1 ON SHEET 2G.

PROPOSED PAVEMENT SCHEDULE

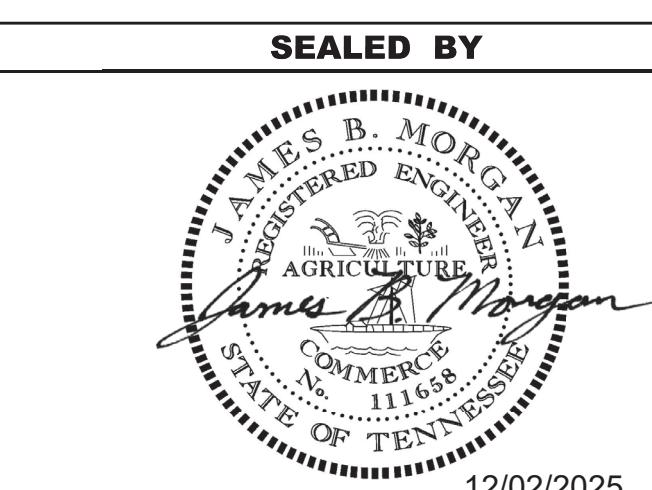
- ① ASPHALTIC CONCRETE SURFACE (HM) 1 $\frac{1}{4}$ " TH. (APPROX. 117.5 LBS/SY)
ITEM NO. 411-03.23 ACS MIX (PG76-22) OGFC
- ② BITUMINOUS PLANT MIX BASE (HM) 1 $\frac{1}{2}$ " TH. (APPROX. 165 LBS/SY)
ITEM NO. 307-03.12 ASPHALT CONCRETE MIX (PG76-22) (BPMB-HM) GRADING C
- ③ TRACKLESS TACK COAT (TC) (APPROX. 0.20 GAL/SY)
ITEM NO. 403-02.01 BITUMINOUS MATERIAL FOR TACK COAT (TC)
- ④ HOT APPLIED TACK COAT (TC) (APPROX. 0.10 - 0.14 GAL/SY)
ITEM NO. 403-02.02 BITUMINOUS MATERIAL FOR TACK COAT (TC)
- ⑤ MINERAL AGGREGATE BASE (SHOULDERS) 2" TH.
ITEM NO. 303-01 MINERAL AGGREGATE, TYPE A BASE, GRADING D

COLD PLANING 1 $\frac{1}{4}$ " TH. (APPROX. 131.3 LBS/SY)
ITEM NO. 415-01.01 COLD PLANING BITUMINOUS PAVEMENT

COLD PLANING 2 $\frac{3}{4}$ " TH. (APPROX. 288.8 LBS/SY)
ITEM NO. 415-01.01 COLD PLANING BITUMINOUS PAVEMENT

ASPHALTIC CONCRETE SURFACE (HM) 1 $\frac{1}{4}$ " TH. (APPROX. 132.5 LBS/SY)
ITEM NO. 411-03.10 ACS MIX (PG76-22) GRADING "D"

SEE SHEET 2G FOR RAMP DETAILS



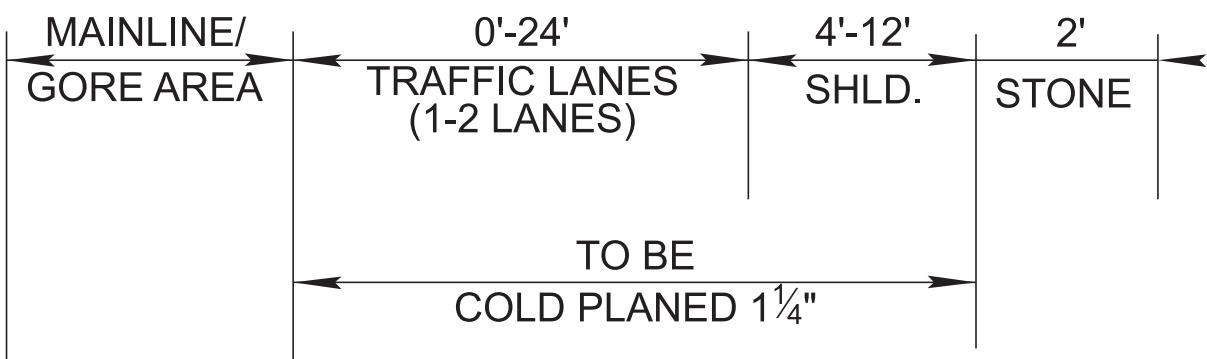
**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS AND PAVEMENT SCHEDULE

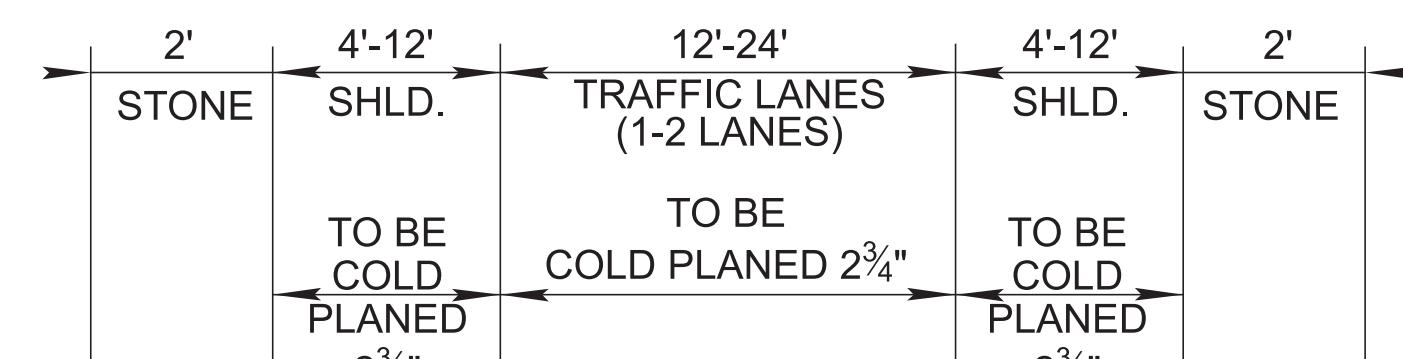
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-40-7(187)	2B1

REV. 12/02/25: ADDED A NOTE.

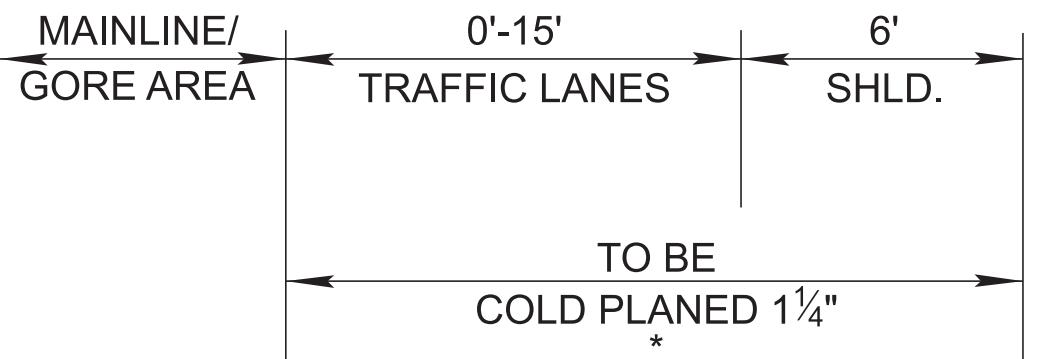
ASPHALT TYPICAL 1



ASPHALT TYPICAL 3

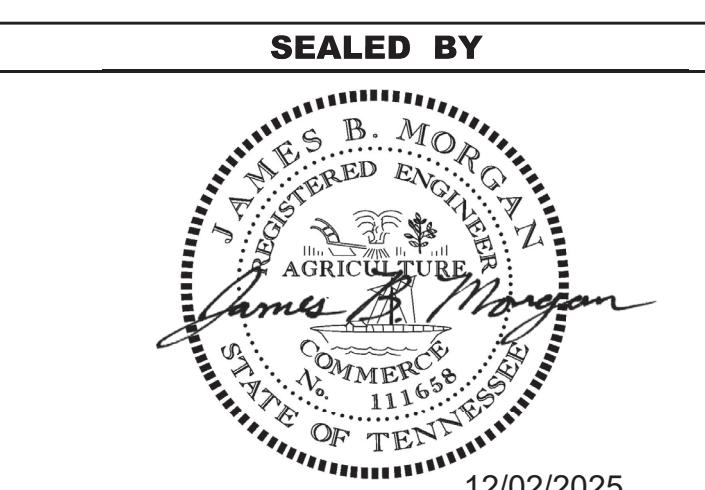
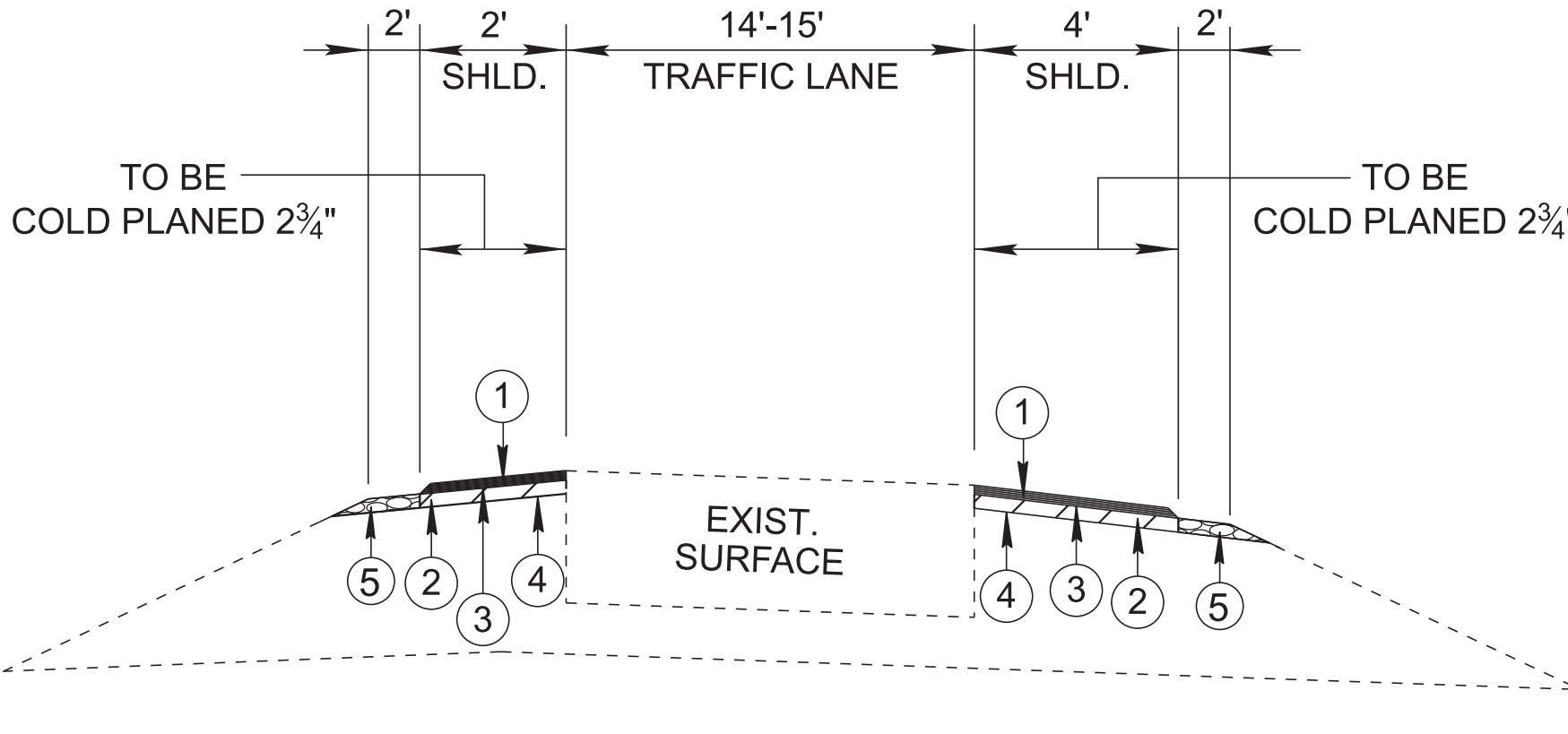


ASPHALT TYPICAL 2



* NOTE: MILL EXISTING ASPHALT ON TOP OF CONCRETE. NO CONCRETE MILLING REQUIRED EXCEPT IN RAMP TRANSITIONS SHOWN IN DETAIL #3 ON SHEET 2G.

CONCRETE TYPICAL 1



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL
SECTIONS

NOTE: SEE PROPOSED PAVEMENT SCHEDULE SHEET 2B.

TYPE	YEAR	PROJECT NO.	sheet no.
RESURF.	2025	NH-I-275-3(137)	2D

REV. 12-02-25: REVISED JOINT SEALANT NOTES.

SPECIAL NOTES

MISCELLANEOUS

- (2) ALL SIGNING AND PAVEMENT MARKINGS ARE TO BE IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (3) THE CONTRACTOR WILL BE REQUIRED TO CUT TREES AND VEGETATION FLUSH WITH THE GROUND WITHIN 35' OF EDGE OF PAVEMENT OR WITHIN 10' IF PROTECTED BEHIND GUARDRAIL. NO ORNAMENTAL AND/OR DECORATIVE TREES SHALL BE CUT OUTSIDE OF THE CLEAR ZONE OR AS DIRECTED BY THE TDOT ENGINEER. ALL TREES AND VEGETATION THAT ARE CUT WILL BE DISPOSED OF AT A SITE APPROVED BY THE TDOT MANAGER. COST TO BE INCLUDED IN THE UNIT BID PRICE FOR CLEARING AND GRUBBING, ITEM NO. 201-01.

PAVEMENT

RESURFACING

- (1) TRAFFIC WILL BE ALLOWED TO TEMPORARILY DRIVE ON THE MILLED SURFACE OF THE ROADWAY UNDER THE FOLLOWING CONDITIONS ONLY:
 - a. THE MILLED SURFACE IS FINE TEXTURED. THE FINE TEXTURE SHALL BE OBTAINED BY A MILLING MACHINE UTILIZING A MILLING HEAD WITH TEETH SPACING 3/8" OR LESS OPERATING AT LESS THAN 80 FEET PER MINUTE.
 - b. THE SURFACE SHALL BE SWEPT AND CLEANED OF ALL LOOSE MATERIALS.
 - c. THE MILLED SURFACE SHALL BE PAVED WITHIN 72 HOURS IF THE CURRENT ADT IS \geq 70,000 OR WITHIN 96 HOURS IF THE CURRENT ADT IS $<$ 70,000.
 - d. RAIN OR INCLEMENT WEATHER IS NOT EXPECTED OR FORECASTED WITHIN 48 HOURS AFTER MILLING.
 - e. ALL APPLICABLE SIGNING IS INSTALLED IN ACCORDANCE WITH THE MUTCD. SIGNING SHALL INCLUDE MOTORCYCLE WARNING SIGNS (W8-15 AND W8-15P) PLACED IN ADVANCE OF ANY MILLED AREAS
 - f. IF MILLED SURFACE BEGINS TO DETERIORATE, PAVING TO COVER UP DETERIORATING MILLED SURFACES SHOULD OCCUR AS DIRECTED BY THE ENGINEER DURING THE NEXT WORKING DAY. IF SEVERE DISTRESS OCCURS, IMMEDIATE RESPONSE WILL BE REQUIRED.
 - g. ONLY ONE LANE IN EACH DIRECTION SHALL HAVE A MILLED SURFACE AT ONE TIME.
- (2) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:
 - A. BROOMING & DEGRASSING SHALL INCLUDE NOTCHING THE GRAVEL SHOULDER PRIOR TO MILLING. THE NOTCH SHALL BE 1.5 IN DEEP AND 2 FT WIDE, OR AS DIRECTED BY THE TDOT PROJECT ENGINEER.
 - B. ALL MATERIAL FROM NOTCHING AND BROOMING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE TDOT PROJECT ENGINEER.
 - C. ALL COSTS ASSOCIATED WITH NOTCHING, PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01-05.
- (3) SURFACE IS TO BE CROWNED AS DIRECTED BY THE TDOT MANAGER.
- (4) ALL SURPLUS PATCHING, JOINT MATERIAL AND CRACK SEALANT IS TO BE REMOVED BEFORE PLACING THE FIRST LAYER OF ASPHALT.
- (7) COLD PLANING IS TO BE DONE IMMEDIATELY PRIOR TO THE COMMENCEMENT OF PAVING OPERATIONS. COLD PLANED SURFACES ARE NOT TO BE LEFT EXPOSED FOR MORE THAN FIVE (5) CALENDAR DAYS BEFORE BEING OVERLAYERED BY THE FIRST COURSE OF ASPHALT. IF NECESSARY, COLD PLANING OPERATIONS ARE TO BE SUSPENDED TO ALLOW PAVING OPERATIONS TO CATCH UP. AFTER COLD PLANING AND PRIOR TO ALLOWING USE BY TRAFFIC, THE CONTRACTOR WILL RAMP ASPHALT AROUND EXISTING UTILITIES TO ELIMINATE HAZARD TO VEHICLES. ALL COLD PLANING REQUIRED ON CITY STREETS OR SIDE ROADS WILL BE PAID FOR AT THE UNIT PRICE BID FOR ITEM NO. 415-01-01.

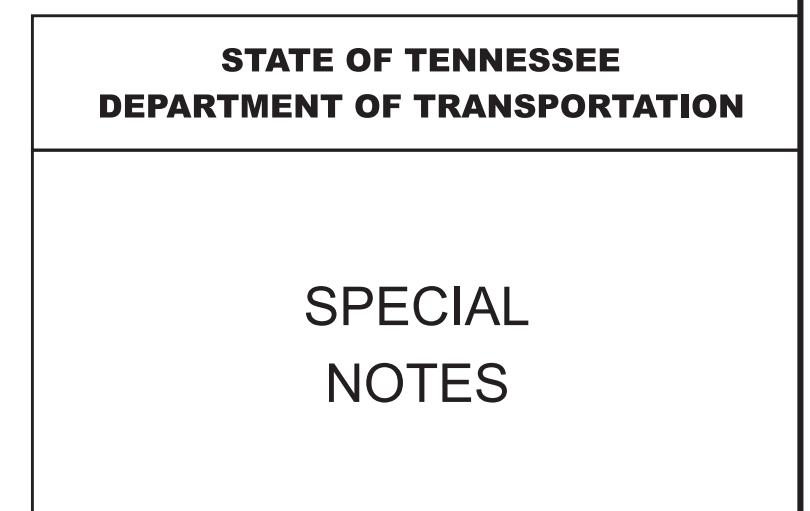
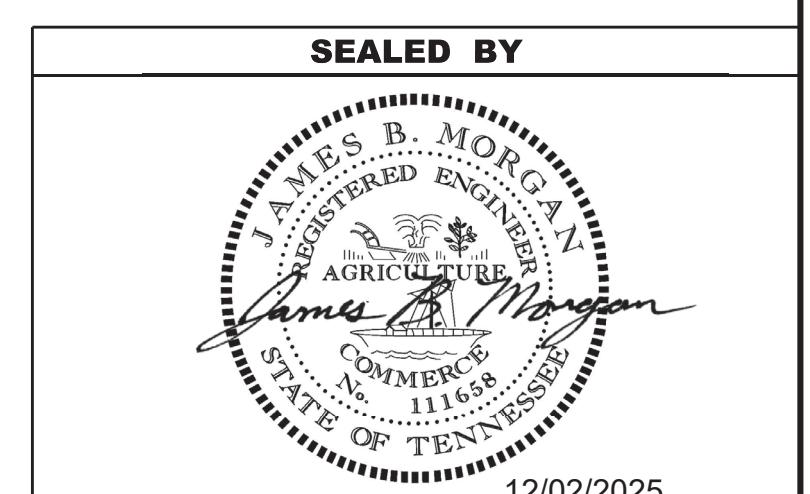
- (8) THE CONTRACTOR SHALL TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT OFF BRIDGE DECKS AS NOT TO DAMAGE THE EXISTING DECK SEALANT AND/OR EXPANSION JOINT MEMBERS (STEEL PLATES, BARS, AND/OR HEADERS). IF DAMAGE DOES OCCUR THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS ON ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE TDOT MANAGER AT NO ADDITIONAL COST.

JOINT SEALANT

- (1) SEQUENCE FOR JOINT SEALANT:
 - a. MILL EXISTING ASPHALT LAYER DOWN TO EXISTING CONCRETE.
 - b. LOCATE EXISTING TRANSVERS CONCRETE JOINTS, PAVE CM MIX, SAW CM MIX $\frac{3}{4}$ " DEPTH ALONG EXISTING CONCRETE JOINTS.
 - c. USING THE ITEM NUMBER 502-08.10, SAWING AND RESEALING (SILICONE SEAL).
 - d. OGFC MIX CAN THEN BE PAVED ABOVE THE CM MIX AS DIRECTED IN THE PLANS.

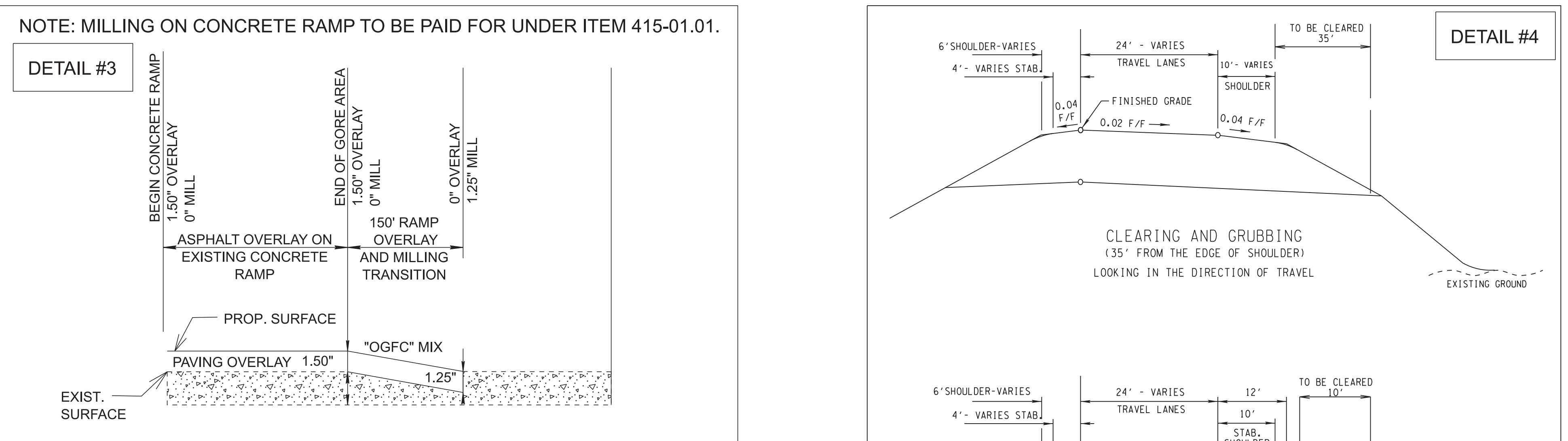
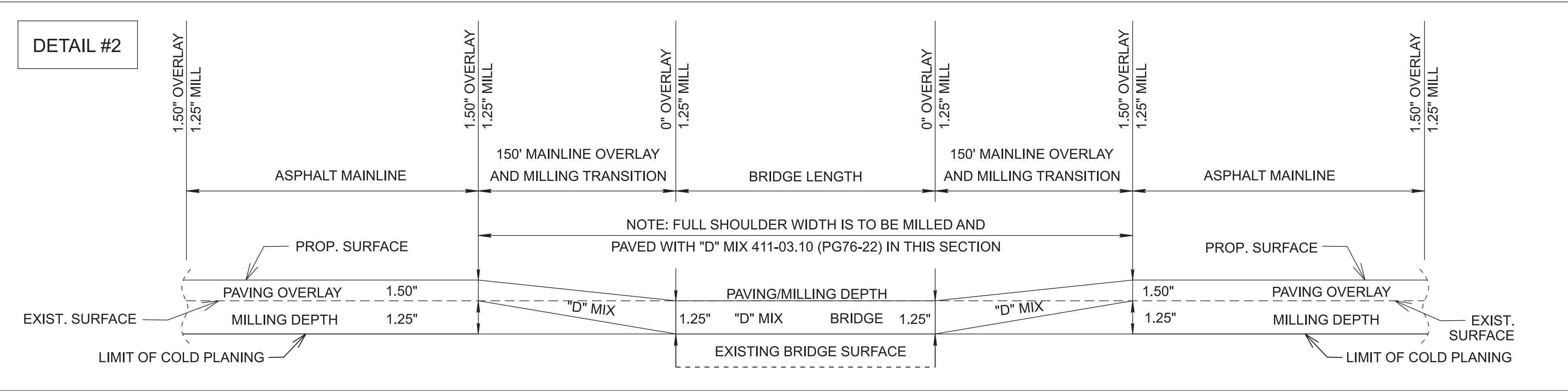
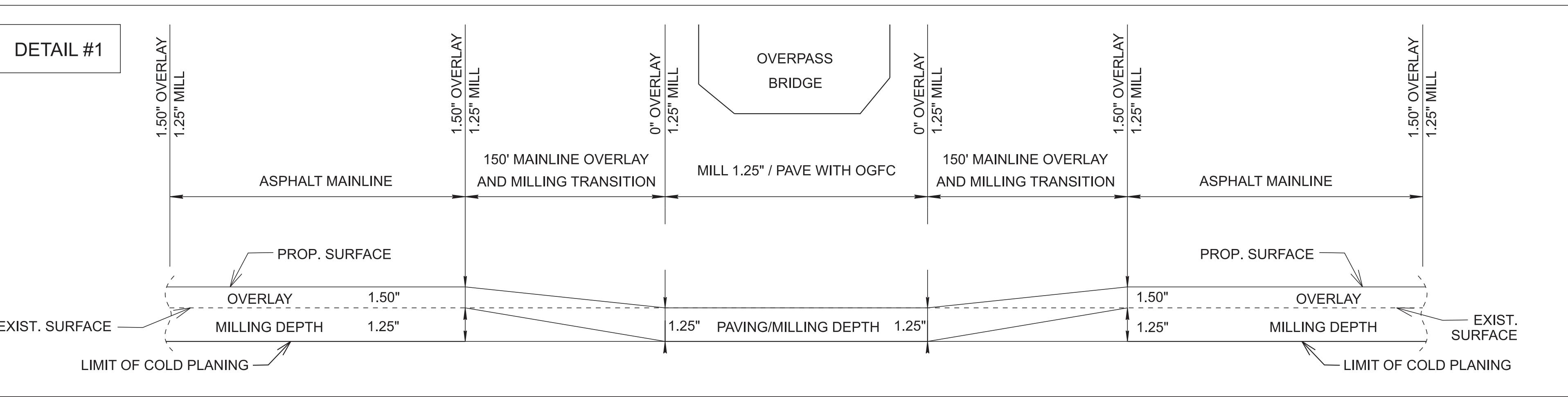
CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (2) ANY WORK REQUIRING LANE CLOSURES INCLUDING PAVEMENT MARKING OPERATIONS SHALL BE AT NIGHT BETWEEN THE HOURS OF 7:00 P.M. AND 6:00 A.M. SUNDAY THROUGH THURSDAY AND 10:00 PM AND 8 AM FRIDAY THROUGH SATURDAY UNLESS OTHERWISE DIRECTED BY THE TDOT MANAGER.
- (4) NO LANE CLOSURES WILL BE ALLOWED ON WEEKENDS (FRIDAY – SUNDAY) WHEN THERE IS A SCHEDULED NASCAR RACE IN BRISTOL UNLESS OTHERWISE APPROVED BY THE TDOT MANAGER AND THE REGIONAL TRAFFIC INCIDENT MANAGEMENT COORDINATOR.
- (5) NO LANE CLOSURES WILL BE ALLOWED ON SATURDAYS WHEN THE UNIVERSITY OF TENNESSEE PLAYS FOOTBALL IN KNOXVILLE UNLESS OTHERWISE APPROVED BY THE TDOT MANAGER AND THE REGIONAL TRAFFIC INCIDENT MANAGEMENT COORDINATOR.

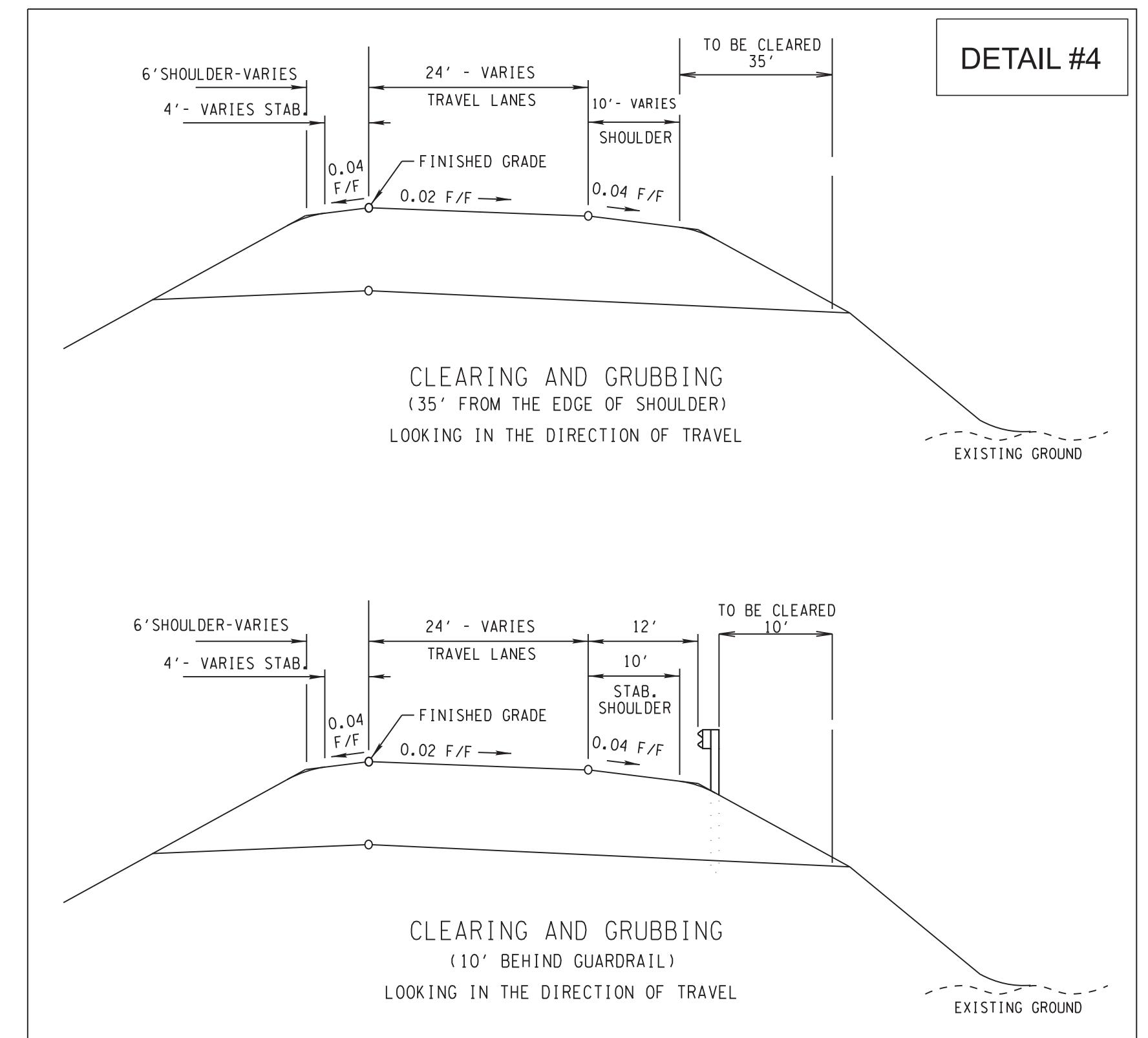


TYPE	YEAR	PROJECT NO.	sheet no.
RESURF.	2025	NH-40-7(187)	2G

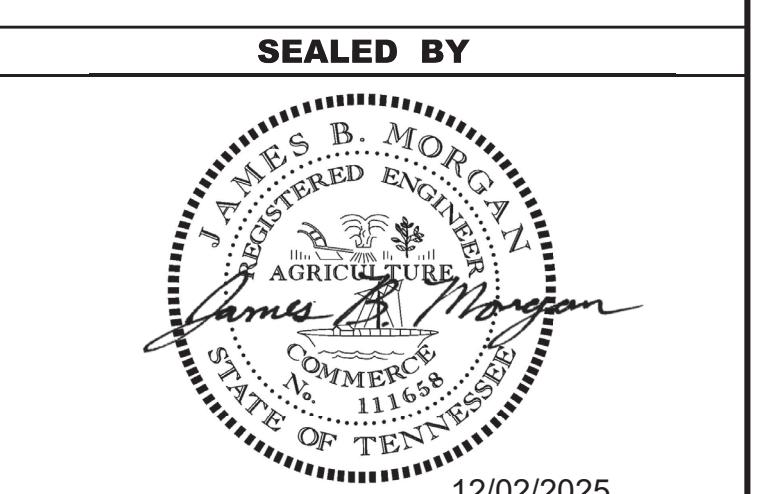
REV 12-2-25: REVISED DETAILS.



THE CONTRACTOR SHOULD TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT OFF EXISTING CONCRETE FOUNDATION AND AT PAVEMENTS ENDS SO AS NOT TO DAMAGE THE EXISTING ELASTOMERIC JOINTS FOR THOSE DO NOT NEED REPAIRING. IF DAMAGE OCCURS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS ON ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE PROJECT ENGINEER AT NO ADDITIONAL COST.



THE CONTRACTOR WILL BE REQUIRED TO CUT TREES AND VEGETATION FLUSH WITH THE GROUND WITHIN 35' OF EDGE OF PAVEMENT OR WITHIN 10' IF PROTECTED BEHIND GUARDRAIL. NO ORNAMENTAL AND/OR DECORATIVE TREES SHALL BE CUT OUTSIDE OF THE CLEAR ZONE OR AS DIRECTED BY THE TDOT ENGINEER. ALL TREES AND VEGETATION THAT ARE CUT WILL BE DISPOSED OF AT A SITE APPROVED BY THE TDOT ENGINEER. COST TO BE INCLUDED IN THE UNIT BID PRICE FOR CLEARING AND GRUBBING, ITEM NO. 201-01.



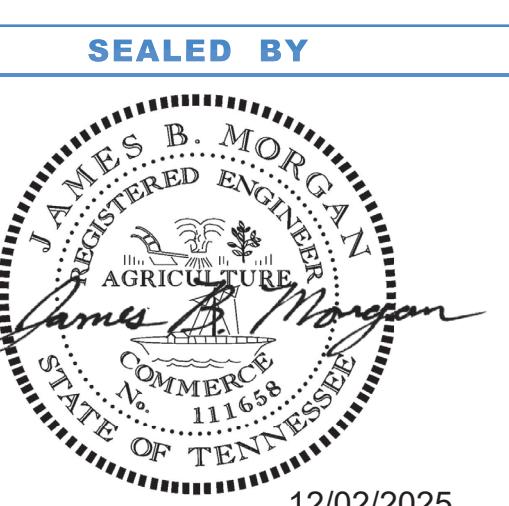
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

DETAIL
SHEETS



TYPE	YEAR	PROJECT NO.	sheet no.
RESURF.	2025	NH-I-275-3(137)	5

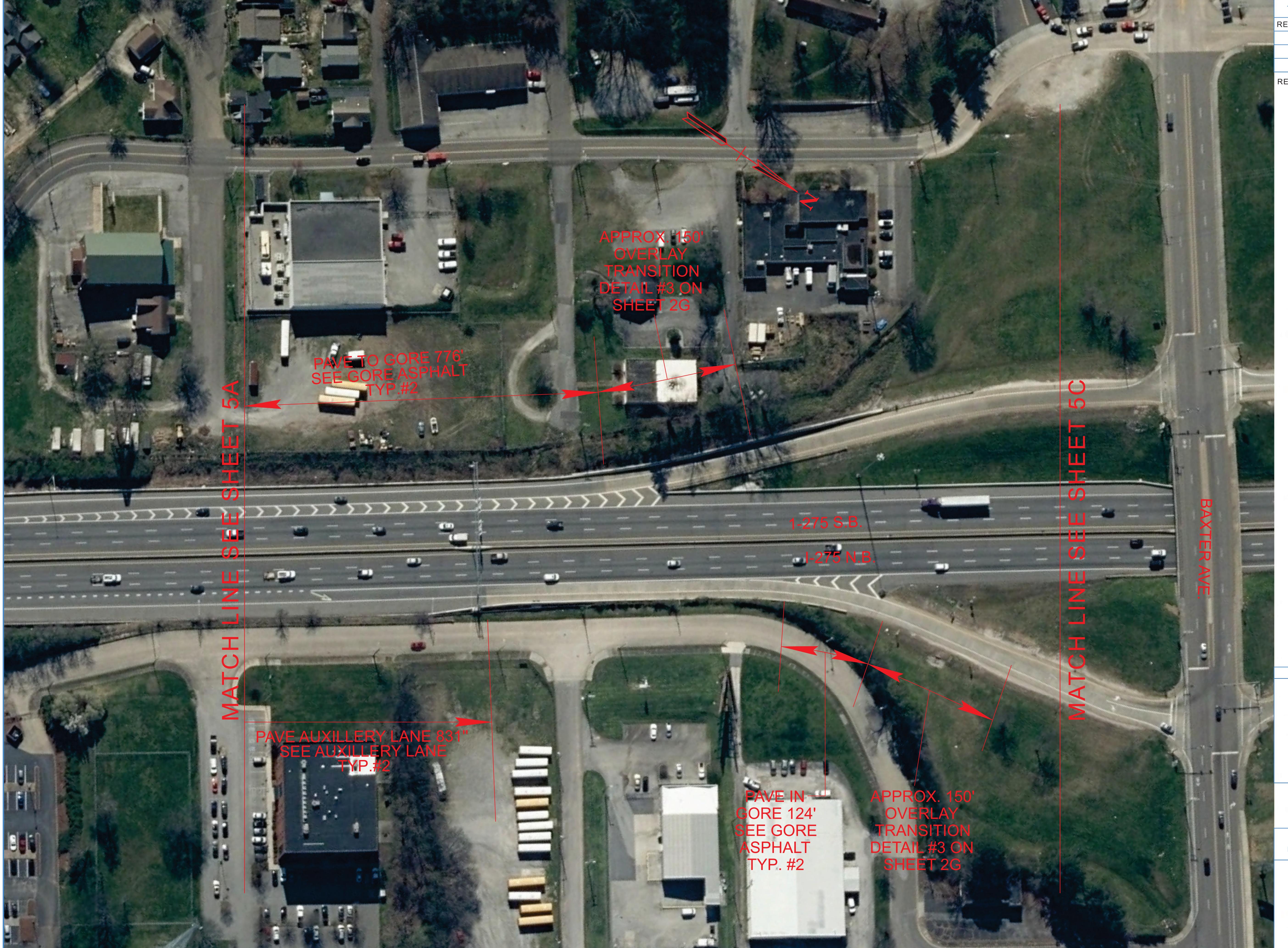
REV 12-2-25: REVISED AERIAL.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

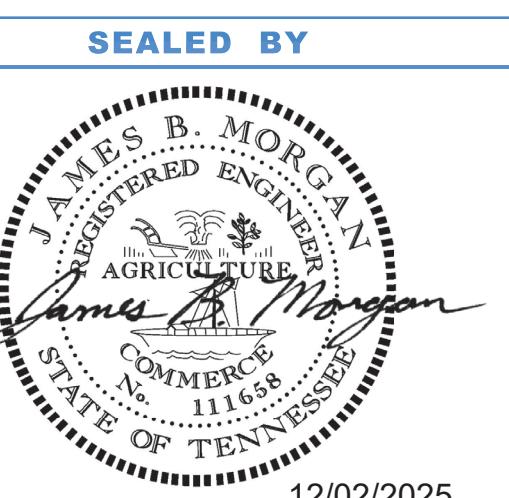
I-40 AT I-275
INTERCHANGE
RAMP DETAILS

SCALE: 1" = 50'



TYPE	YEAR	PROJECT NO.	sheet no.
RESURF.	2025	NH-I-275-3(137)	5B

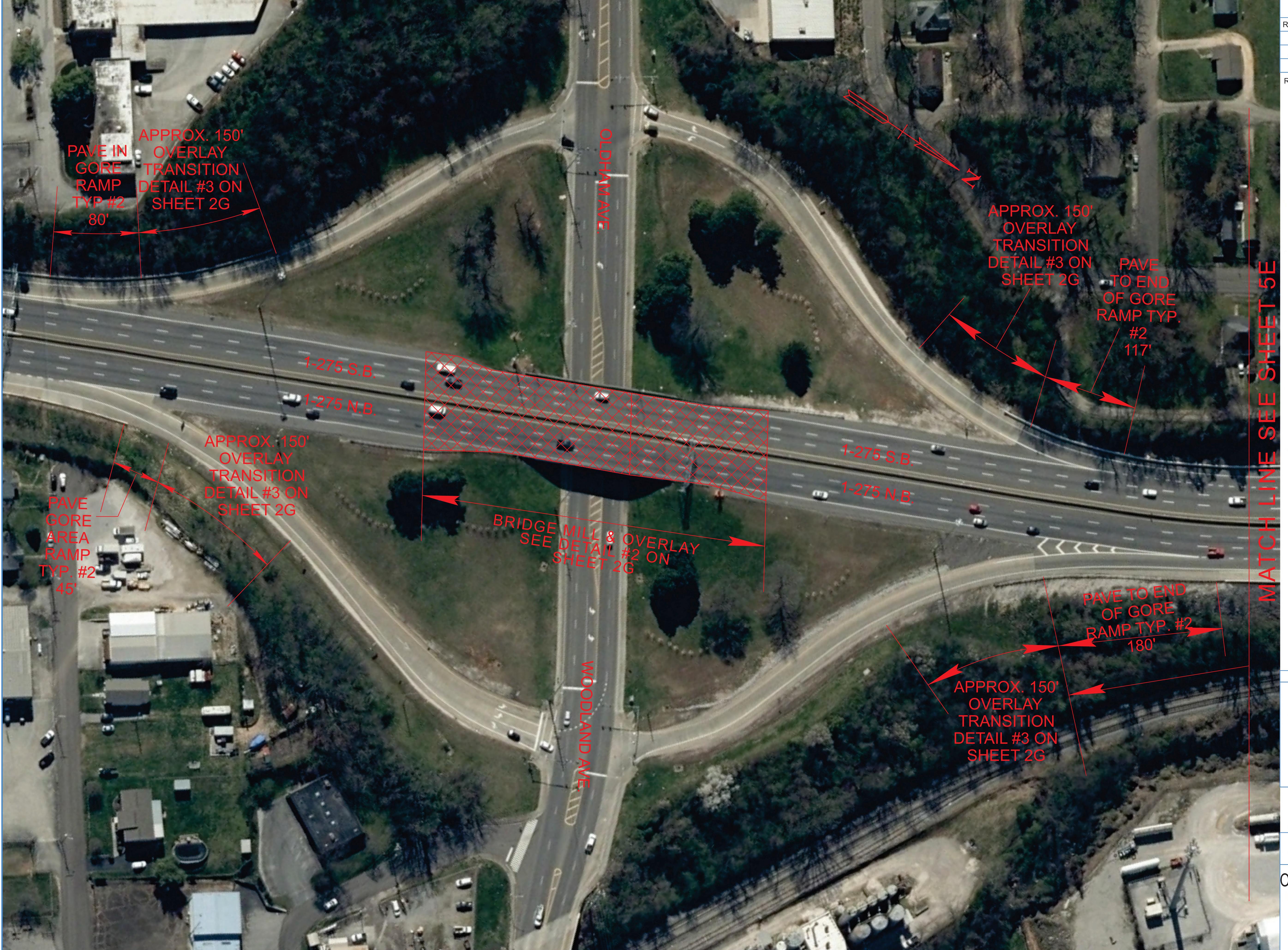
REV 12-2-25: REVISED AERIAL.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

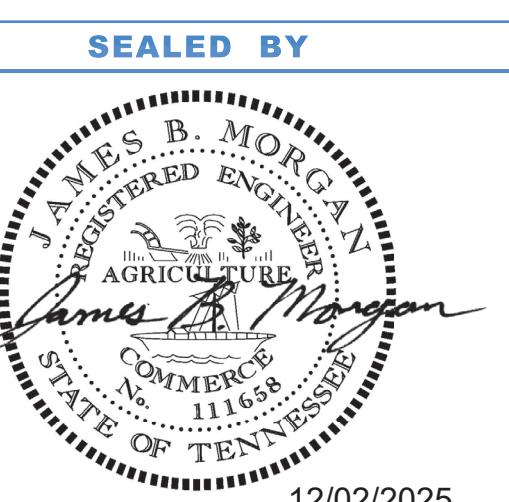
BAXTER AVE.
INTERCHANGE
RAMP DETAILS

SCALE: 1"= 50'



TYPE	YEAR	PROJECT NO.	sheet no.
RESURF.	2025	NH-I-275-3(137)	5D

REV 12-2-25: REVISED AERIAL.

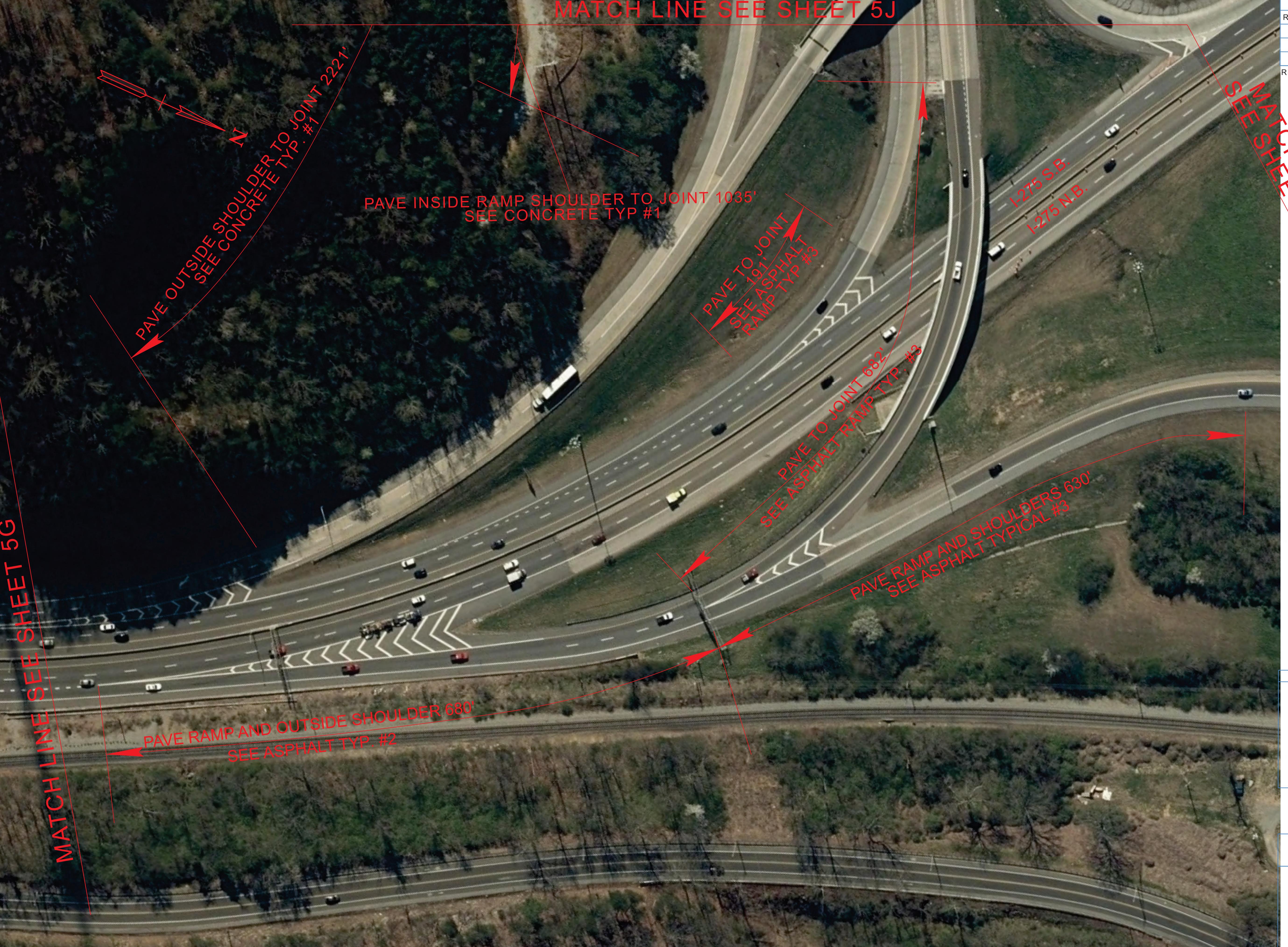


SEALED BY
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

OLDHAM & WOODLAND AVE.
INTERCHANGE
RAMP DETAILS

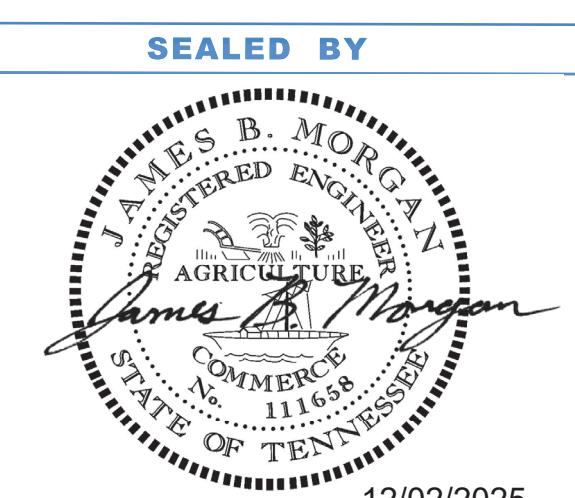
SCALE: 1" = 50'

MATCH LINE SEE SHEET 5J



TYPE	YEAR	PROJECT NO.	sheet no.
RESURF.	2025	NH-I-275-3(137)	5H

REV 12-2-25: REVISED AERIAL.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

I-275, I-640 & I-75
INTERCHANGE
RAMP DETAILS

SCALE: 1" = 50'